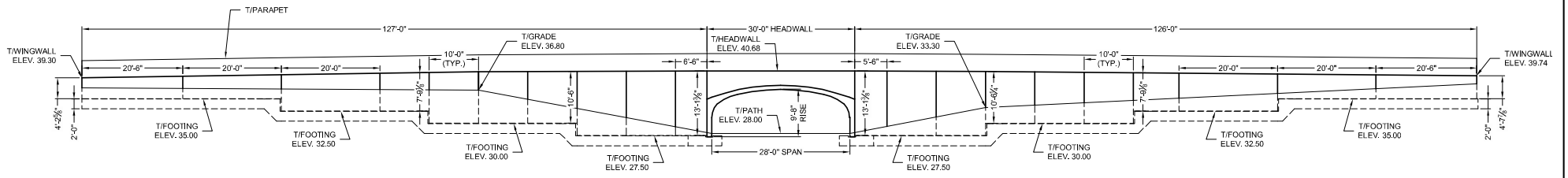
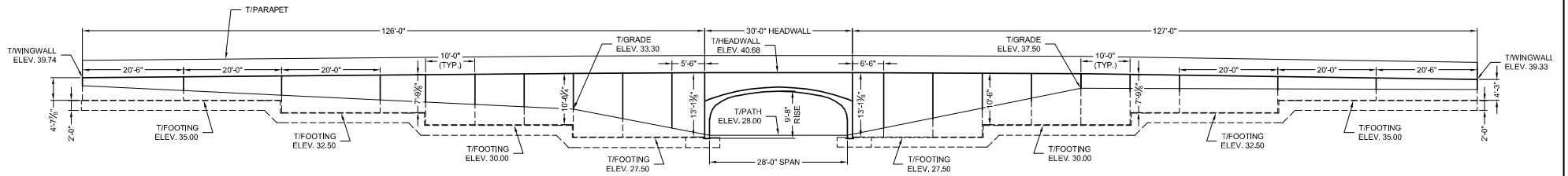


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EAST END ELEVATION



WEST END ELEVATION

PRELIMINARY
NOT FOR CONSTRUCTION

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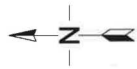
CONISPAN
BRIDGE SYSTEMS

PROPOSAL
DRAWING

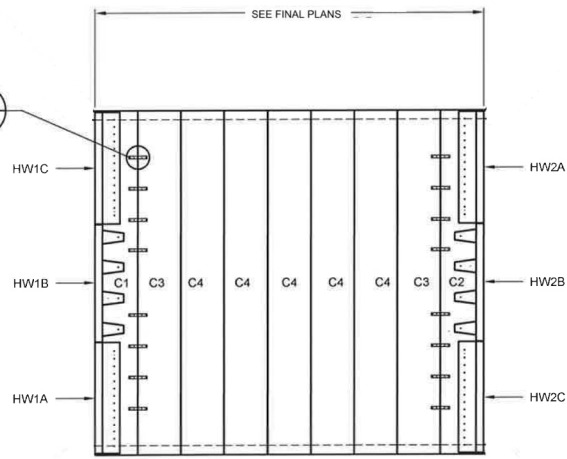
RIVERLAND BOULEVARD
RIVERLAND PASEO OVERPASS
ST. LUCIE COUNTY, FLORIDA

PROJECT No.:	SEQ. No.:	DATE:
DESIGNED:	DRAWN:	3/4/2021
KMH	KMH	
CHECKED:	APPROVED:	
SHEET NO.:	1	OF 1

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DETAIL 19
S16



KEY PLAN

MICHAEL G. CARFAGNO
P.E. LICENSE NO. 70292
CONTECH ENGINEERED SOLUTIONS LLC
9025 CENTRE POINTE DRIVE, SUITE 400
WEST CHESTER, OH 45089
CERTIFICATE OF AUTHORIZATION NO. 30094

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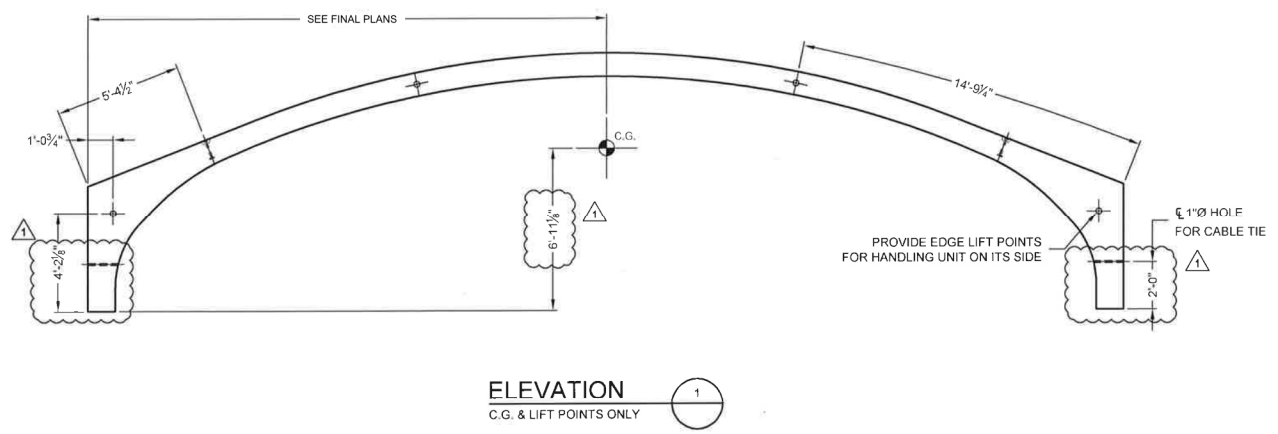
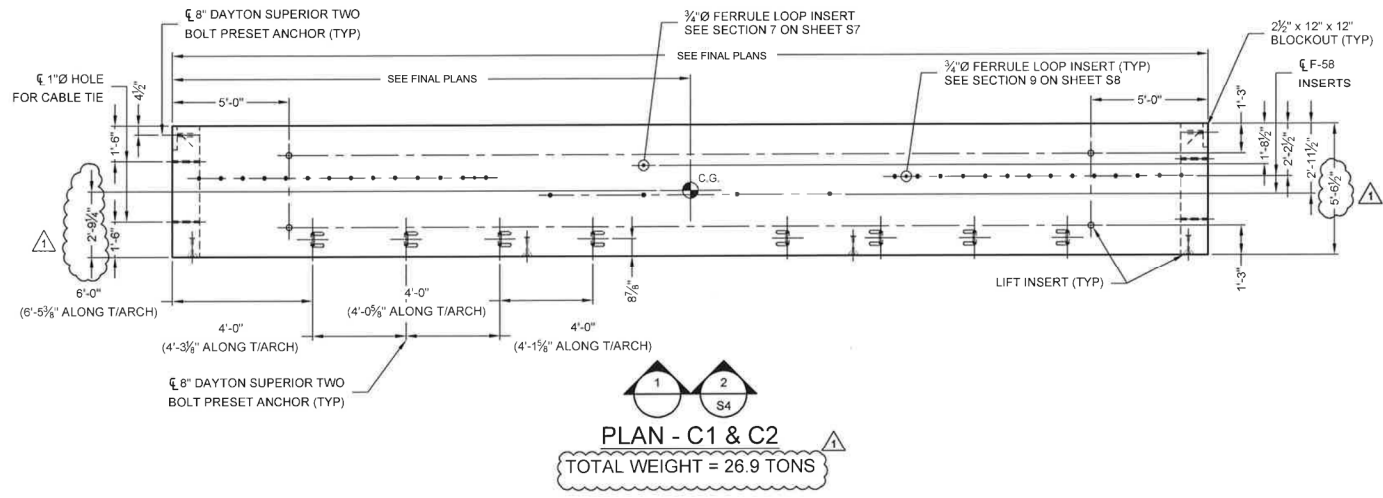
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CONSPAN
BRIDGE SYSTEMS

FABRICATION
DRAWING

PROJECT No: 531323	SEQ. No: 010	DATE:
DESIGNED MGC	DRAWN ALK	
CHECKED KMH	APPROVED MGC	
SHEET NO: S1 OF S18		

NOTES:
 - ALL EDGES OF PRECAST TO HAVE A 3/8" CHAMFER
 - SEE SHEET S2 FOR BRIDGE UNIT DIMENSIONS
 - INSTALL TIE RODS BEFORE UNIT IS SET UPRIGHT & LEAVE IN PLACE UNTIL UNIT IS GROUTED INTO FINAL POSITION
 - BRIDGE UNIT WILL NOT HANG LEVEL. ADJUST CABLE LENGTHS AS REQUIRED



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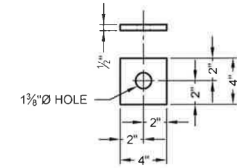
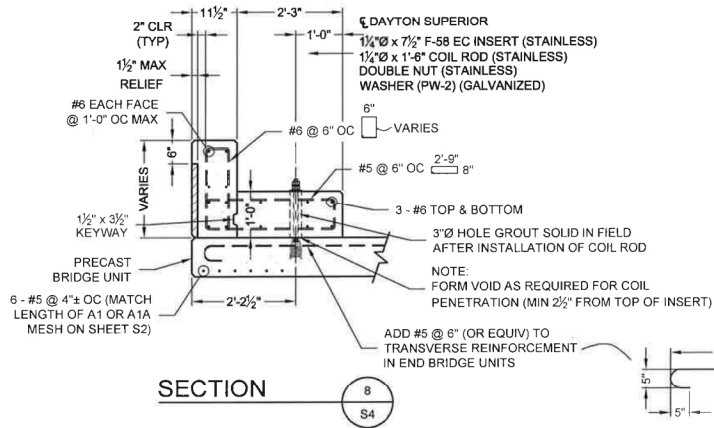
MARK	DATE	REVISION DESCRIPTION	BY
1	1/2/2018	REVISED BRIDGE RISE	MGC

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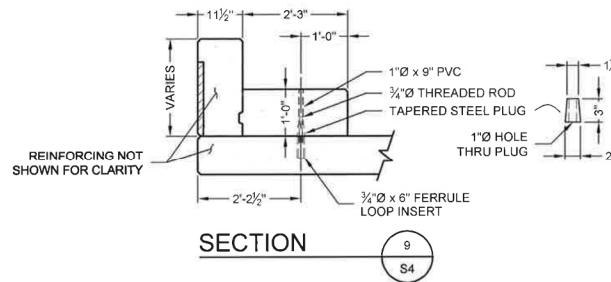
CONISPAN
 BRIDGE SYSTEMS
 FABRICATION
 DRAWING

PROJECT No. 531323	SEQ. No. 010	DATE
DESIGNED MGC	DRAWN ALK	
CHECKED KMH	APPROVED MGC	
SHEET NO. S3 OF S18		

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PW-2
($\frac{1}{2}$ " WASHER $\frac{1}{2}$ " x 4" x 4")
(GALVANIZED AS PER ASTM A123)



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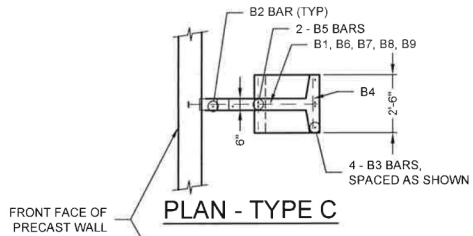
MARK	DATE	REVISION DESCRIPTION	BY
1	1/2/2018	NO CHANGES THIS SHEET	MGC

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CONISPAN
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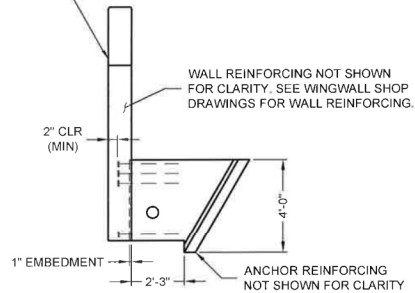
PROJECT No: 531323	SEQ. No: 010	DATE:
DESIGNED MGC	DRAWN ALK	
CHECKED KMH	APPROVED MGC	
SHEET NO.: S8 OF S18		

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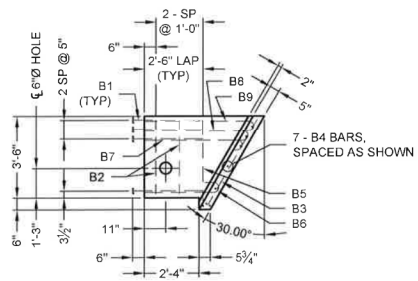


BAR LIST - TYPE C						
MARK	QTY	SIZE	A	TYPE	LENGTH	FINISH
B1	4	#6	3'-0"	3	---	EPOXY*
B2	2	#5	---	STR	3'-2"	BLACK
B3	4	#5	---	STR	4'-3"	BLACK
B4	7	#5	---	STR	2'-2"	BLACK
B5	2	#5	3'-8"	2	---	BLACK
B6	1	#5	3'-2"	1	---	BLACK
B7	1	#5	4'-2"	1	---	BLACK
B8	1	#5	4'-5"	1	---	BLACK
B9	1	#5	4'-8"	1	---	BLACK

NOTE: "STR" DENOTES STRAIGHT BAR. STANDARD CLEARANCE = 2"

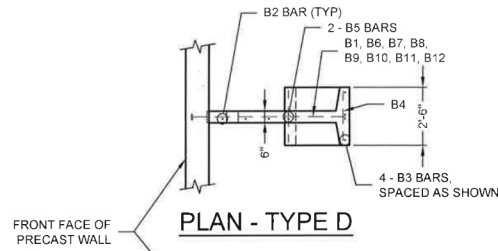


SECTION - TYPE C



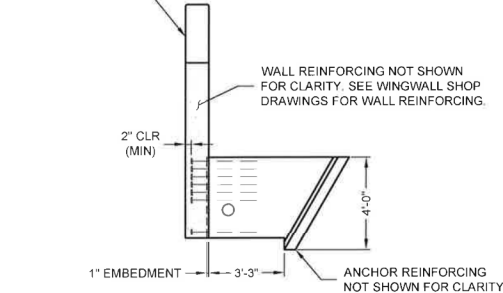
PRECAST ANCHOR TYPE C

TOTAL WEIGHT = .896 TONS

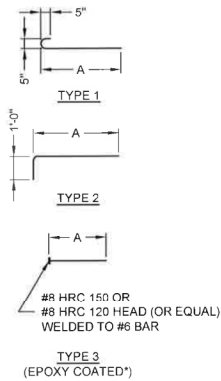


BAR LIST - TYPE D						
MARK	QTY	SIZE	A	TYPE	LENGTH	FINISH
B1	7	#6	3'-0"	3	---	EPOXY*
B2	3	#5	---	STR	3'-2"	BLACK
B3	4	#5	---	STR	4'-3"	BLACK
B4	9	#5	---	STR	2'-2"	BLACK
B5	2	#5	3'-8"	2	---	BLACK
B6	1	#5	4'-2"	1	---	BLACK
B7	1	#5	4'-8"	1	---	BLACK
B8	1	#5	4'-10"	1	---	BLACK
B9	1	#5	5'-1"	1	---	BLACK
B10	1	#5	5'-3"	1	---	BLACK
B11	1	#5	5'-5"	1	---	BLACK
B12	1	#5	5'-8"	1	---	BLACK

NOTE: "STR" DENOTES STRAIGHT BAR. STANDARD CLEARANCE = 2"

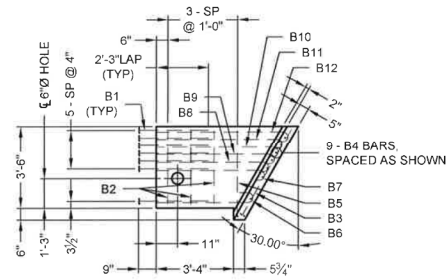


SECTION - TYPE D



*NOTE: EPOXY COATING IS NOT REQUIRED ON HEADED ENDS OF TYPE 3 BARS, BUT WILL NOT BE DETRIMENTAL IF PROVIDED.

*NOTE: HRC HEADS PROVIDED BY HEADED REINFORCEMENT CORP.



PRECAST ANCHOR TYPE D

TOTAL WEIGHT = 1.028 TONS

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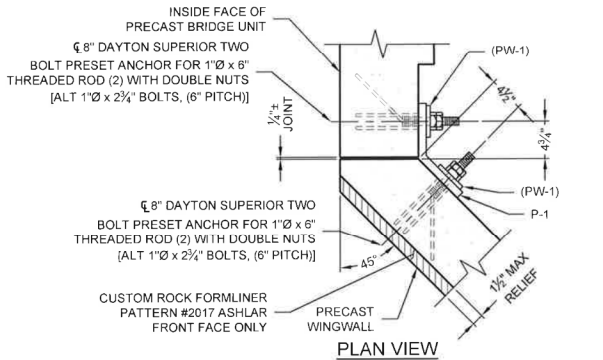
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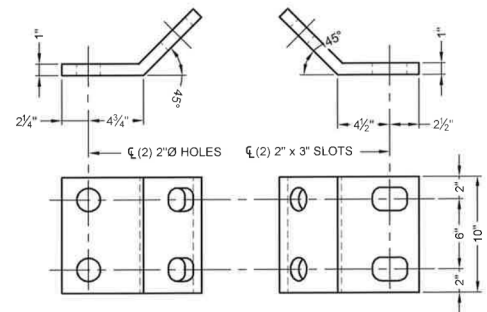
CONSPAN
BRIDGE SYSTEMS
FABRICATION
DRAWING

PROJECT No.	SEQ. No.	DATE
531323	010	
DESIGNED: MGC	DRAWN: ALK	
CHECKED: KMH	APPROVED: MGC	
SHEET NO. S14 OF S18		

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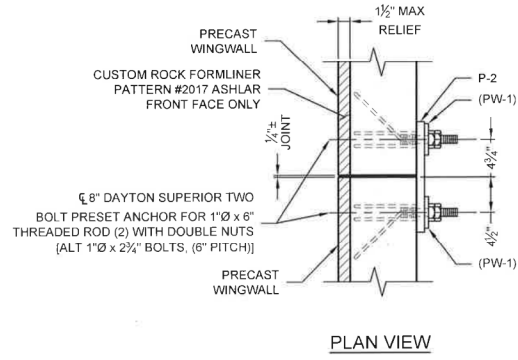
TYPICAL CONNECTION DETAIL - P-1



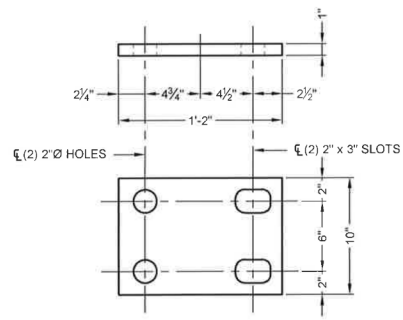
UNIT LEG WINGWALL LEG

P-1
(\bar{R} , 1" x 14" x 10")
(GALVANIZED AS PER ASTM A123)

PLATE P-1	
TOTAL REQUIRED = 12 (4) PW-1 REQ'D PER PLATE	



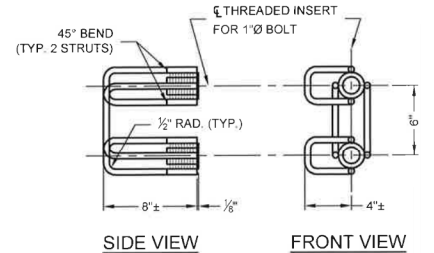
TYPICAL CONNECTION DETAIL - P-2



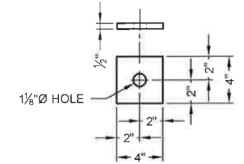
P-2

(\bar{R} , 1" x 14" x 10")
(GALVANIZED AS PER ASTM A123)

PLATE P-2	
TOTAL REQUIRED = 16 (4) PW-1 REQ'D PER PLATE	



**DAYTON SUPERIOR
TWO BOLT PRESET ANCHOR**



PW-1
(\bar{R} WASHER, 1/2" x 4" x 4")
(GALVANIZED AS PER ASTM A123)

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MICHAEL G. CARFAGNO
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PROJECT No.	513123	SEQ. No.	010	DATE:
DESIGNED	MGC	DRAWN	ALK	
CHECKED	KMH	APPROVED	MGC	
SHEET NO.		S15 OF S18		

SPECIFICATIONS FOR MANUFACTURE AND INSTALLATION OF CON/SPAN® BRIDGE SYSTEMS

- 1. DESCRIPTION**
- 1.1. TYPE - THIS WORK SHALL CONSIST OF FURNISHING AND CONSTRUCTION OF CON/SPAN BRIDGE SYSTEM IN ACCORDANCE WITH THESE SPECIFICATIONS AND IN REASONABLY CLOSE CONFORMANCE WITH THE LINES, GRADES, DESIGN AND DIMENSIONS SHOWN ON THE PLANS OR AS ESTABLISHED BY THE ENGINEER. IN SITUATIONS WHERE TWO OR MORE SPECIFICATIONS APPLY TO THIS WORK, THE MOST STRINGENT REQUIREMENTS SHALL GOVERN.
- 1.2. DESIGNATION - PRECAST REINFORCED CONCRETE CON/SPAN® BRIDGE UNITS MANUFACTURED IN ACCORDANCE WITH THIS SPECIFICATION SHALL BE DESIGNATED BY SPAN AND RISE. PRECAST REINFORCED CONCRETE WINGWALLS AND HEADWALLS MANUFACTURED IN ACCORDANCE WITH THIS SPECIFICATION SHALL BE DESIGNATED BY SPAN AND RISE. PRECAST REINFORCED CONCRETE EXPRESS™ FOUNDATION UNITS MANUFACTURED IN ACCORDANCE WITH THIS SPECIFICATION SHALL BE DESIGNATED BY LENGTH, HEIGHT AND WIDTH.
- 2. DESIGN**
- 2.1. SPECIFICATIONS - THE PRECAST ELEMENTS ARE DESIGNED IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE SPECIFICATION 7TH EDITION, ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 2015. A MINIMUM OF ONE FOOT OF COVER ABOVE THE CROWN OF THE BRIDGE UNITS IS REQUIRED IN THE INSTALLED CONDITION. (UNLESS NOTED OTHERWISE ON THE SHOP DRAWINGS AND DESIGNED ACCORDINGLY.)
- 3. MATERIALS**
- 3.1. CONCRETE - THE CONCRETE FOR THE PRECAST ELEMENTS SHALL BE AIR-ENTRAINED WHEN INSTALLED IN AREAS SUBJECT TO FROST. THE CONCRETE SHALL BE A MIXTURE OF PORTLAND CEMENT, FINE AND COARSE AGGREGATES, ADMIXTURES AND WATER. AIR-ENTRAINED CONCRETE SHALL CONTAIN 6 ± 2 PERCENT AIR. THE MIXTURE SHALL BE PROPORTIONED TO CONFORM TO AASHTO M154. THE MINIMUM CONCRETE COMPRESSIVE STRENGTH SHALL BE AS SHOWN ON THE SHOP DRAWINGS.
- 3.1.1. PORTLAND CEMENT - SHALL CONFORM TO THE REQUIREMENTS OF ASTM SPECIFICATIONS C150-TYPE I OR TYPE II CEMENT.
- 3.1.2. COARSE AGGREGATE - SHALL CONSIST OF STONE HAVING A MAXIMUM SIZE OF 1 INCH. AGGREGATE SHALL MEET REQUIREMENTS FOR ASTM SPECIFICATION C33.
- 3.1.3. WATER REDUCING ADMIXTURE - THE MANUFACTURER MAY SUBMIT FOR APPROVAL BY THE ENGINEER, A WATER-REDUCING ADMIXTURE FOR THE PURPOSE OF INCREASING WORKABILITY AND REDUCING THE WATER REQUIREMENT FOR THE CONCRETE.
- 3.1.4. BALANCE OF THE MIXTURE - IN ADDITION TO THE MIX OF CALCIUM CHLORIDE OR ADMIXTURES CONTAINING CALCIUM CHLORIDE WILL NOT BE PERMITTED.
- 3.1.5. MIXTURE - THE AGGREGATES, CEMENT AND WATER SHALL BE PROPORTIONED AND MIXED IN A BATCH MIXER TO PRODUCE A HOMOGENEOUS CONCRETE MEETING THE STRENGTH REQUIREMENTS OF THIS SPECIFICATION. THE PROPORTION OF PORTLAND CEMENT IN THE MIXTURE SHALL NOT BE LESS THAN 964 POUNDS (6 SACKS) PER CUBIC YARD OF CONCRETE.
- 3.2. STEEL**
- 3.2.1. THE MINIMUM STEEL YIELD STRENGTH SHALL BE 60,000 PSI. UNLESS OTHERWISE NOTED ON THE SHOP DRAWINGS.
- 3.2.2. REINFORCING STEEL FOR THE PRECAST ELEMENTS SHALL BE FABRICATED AND PLACED IN ACCORDANCE WITH THE DETAILED SHOP DRAWINGS SUBMITTED BY THE MANUFACTURER.
- 3.2.3. REINFORCEMENT SHALL CONSIST OF WELDED WIRE REINFORCING CONFORMING TO ASTM SPECIFICATION A 108, OR DEFORMED BILLET STEEL BARS CONFORMING TO ASTM SPECIFICATION A 615, GRADE 60, LONGITUDINAL DISTRIBUTION REINFORCEMENT MAY CONSIST OF WELDED WIRE FABRIC OR DEFORMED BILLET STEEL BARS.
- 3.3. STEEL HARDWARE**
- 3.3.1. BOLTS AND THREADED RODS FOR WINGWALL CONNECTIONS SHALL CONFORM TO ASTM A 307. NUTS SHALL CONFORM TO AASHTO M252 (ASTM A194) GRADE 2H. ALL BOLTS, THREADED RODS AND NUTS USED IN WINGWALL CONNECTIONS SHALL BE MECHANICALLY ZINC COATED IN ACCORDANCE WITH ASTM B695 CLASS 50.
- 3.3.2. STRUCTURAL STEEL FOR WINGWALL CONNECTION PLATES AND PLATE WASHERS SHALL CONFORM TO AASHTO M 270 (ASTM A 709) GRADE 36 AND SHALL BE HOT DIP GALVANIZED AS PER AASHTO M111 (ASTM A123).
- 3.3.3. INSERTS FOR WINGWALLS SHALL BE FABRICATED FROM TWO-BOLT PRESET WINGWALL ANCHORS AS MANUFACTURED BY DAYTON SUPERIOR CONCRETE ACCESSORIES, MIAMISBURG, OHIO, (800) 745-3700 AND SHALL BE MECHANICALLY ZINC COATED IN ACCORDANCE WITH ASTM B695 CLASS 50.
- 3.3.4. FERRULE LOOP INSERTS SHALL BE F-64 FERRULE LOOP INSERTS AS MANUFACTURED BY DAYTON SUPERIOR CONCRETE ACCESSORIES, MIAMISBURG, OHIO, (800) 745-3700.
- 3.3.5. BOLTS USED IN ATTACHED HEADWALL CONNECTIONS SHALL BE ASTM A307.
- 3.3.6. INSERTS FOR DETACHED HEADWALL CONNECTIONS SHALL HAVE THE SAME ANNEALING TREATMENT AS MANUFACTURED BY DAYTON SUPERIOR

- CONCRETE ACCESSORIES, MIAMISBURG, OHIO, (800) 745-3700. COIL RODS AND NUTS USED IN HEADWALL CONNECTIONS SHALL BE AIR-ENTRAINED TYPE 304 STAINLESS STEEL WASHERS USED IN HEADWALL CONNECTIONS SHALL BE EITHER AISI TYPE 304 STAINLESS STEEL PLATE WASHERS OR AASHTO M207 (ASTM A 709) GRADE 36 PLATE WASHERS HOT DIP GALVANIZED AS PER AASHTO M111 (ASTM A123).
- 3.3.7. MECHANICAL SPLICES OF REINFORCING BARS SHALL BE MADE USING THE DOWEL BAR SPICER SYSTEM AS MANUFACTURED BY DAYTON SUPERIOR CONCRETE ACCESSORIES, MIAMISBURG, OHIO, (800) 745-3700, AND SHALL CONSIST OF THE DOWEL BAR SPICER SYSTEM (DB-S48) AND DOWEL-IN (DI).
- 4. MANUFACTURE OF PRECAST ELEMENTS - SUBJECT TO THE PROVISIONS OF SECTIONS 5, BELOW, THE PRECAST ELEMENT DIMENSION AND REINFORCEMENT DETAILS SHALL BE AS PRESCRIBED IN THE PLAN AND SHOP DRAWINGS PROVIDED BY THE MANUFACTURER.**
- 4.1. FORMS - THE FORMS USED IN MANUFACTURE SHALL BE SUFFICIENTLY RIGID AND ACCURATE TO MAINTAIN THE REQUIRED PRECAST ELEMENT DIMENSIONS WITHIN THE PERMISSIBLE VARIATIONS GIVEN IN SECTION 5 OF THESE SPECIFICATIONS. ALL CASTING SURFACES SHALL BE OF A SMOOTH MATERIAL.
- 4.2. PLACEMENT OF REINFORCEMENT
- 4.2.1. PLACEMENT OF REINFORCEMENT IN PRECAST BRIDGE UNITS - THE COVER OF CONCRETE OVER THE OUTSIDE CIRCUMFERENTIAL REINFORCEMENT SHALL BE 2" MINIMUM. THE COVER OF CONCRETE OVER THE INSIDE CIRCUMFERENTIAL REINFORCEMENT SHALL BE 1/2" MINIMUM, UNLESS OTHERWISE NOTED ON THE SHOP DRAWINGS. THE CLEAR DISTANCE OF THE END CIRCUMFERENTIAL WIRE OR WIRE FABRIC SHALL NOT BE MORE THAN 2" FROM THE ENDS OF EACH SECTION. REINFORCEMENT SHALL BE ASSEMBLED UTILIZING SINGLE OR MULTIPLE LAYERS OF WELDED WIRE FABRIC (NOT TO EXCEED 3 LAYERS), SUPPLEMENTED WITH A SINGLE LAYER OF DEFORMED BILLET STEEL BARS, WHEN NECESSARY.
- 4.2.2. WELDED WIRE FABRIC SHALL MEET THE REQUIREMENTS OF CIRCUMFERENTIAL AND LONGITUDINAL WIRES MEETING THE SPACING REQUIREMENTS OF 4.3, BELOW, AND SHALL CONTAIN SUFFICIENT LONGITUDINAL WIRE OR WIRE FABRIC OR DEFORMED BILLET-STEEL BARS AND SHALL MEET THE SPACING REQUIREMENTS OF 4.3, BELOW. THE LONGITUDINAL REINFORCEMENT SHALL BE WELDED WIRE FABRIC OR DEFORMED BILLET-STEEL BARS AND SHALL MEET THE SPACING REQUIREMENTS OF 4.3, BELOW. THE CLEAR DISTANCE FROM THE END OF EACH PRECAST ELEMENT TO THE END OF REINFORCING STEEL SHALL NOT BE LESS THAN 15% NOR MORE THAN 3". REINFORCEMENT SHALL BE ASSEMBLED UTILIZING A SINGLE LAYER OF WELDED WIRE FABRIC OR A SINGLE LAYER OF DEFORMED BILLET STEEL BARS. WELDED WIRE FABRIC SHALL MEET THE TRANSVERSE AND LONGITUDINAL WIRES MEETING THE SPACING REQUIREMENTS OF 4.3, BELOW, AND SHALL CONTAIN SUFFICIENT LONGITUDINAL WIRES EXTENDING THROUGH THE ELEMENT TO MAINTAIN THE SHAPE AND POSITION OF THE REINFORCEMENT. LONGITUDINAL REINFORCEMENT MAY BE WELDED WIRE FABRIC OR DEFORMED BILLET STEEL BARS AND SHALL MEET THE SPACING REQUIREMENTS OF 4.3, BELOW.
- 4.2.3. PLACEMENT OF REINFORCEMENT FOR PRECAST WINGWALLS AND HEADWALLS - THE COVER OF CONCRETE OVER THE LONGITUDINAL AND TRANSVERSE REINFORCEMENT SHALL BE 2" MINIMUM. THE CLEAR DISTANCE FROM THE END OF EACH PRECAST ELEMENT TO THE END OF REINFORCING STEEL SHALL NOT BE LESS THAN 15% NOR MORE THAN 3". REINFORCEMENT SHALL BE ASSEMBLED UTILIZING A SINGLE LAYER OF WELDED WIRE FABRIC OR A SINGLE LAYER OF DEFORMED BILLET STEEL BARS. WELDED WIRE FABRIC SHALL MEET THE TRANSVERSE AND LONGITUDINAL WIRES MEETING THE SPACING REQUIREMENTS OF 4.3, BELOW, AND SHALL CONTAIN SUFFICIENT LONGITUDINAL WIRES EXTENDING THROUGH THE ELEMENT TO MAINTAIN THE SHAPE AND POSITION OF THE REINFORCEMENT. LONGITUDINAL REINFORCEMENT MAY BE WELDED WIRE FABRIC OR DEFORMED BILLET STEEL BARS AND SHALL MEET THE SPACING REQUIREMENTS OF 4.3, BELOW.
- 4.2.4. PLACEMENT OF REINFORCEMENT FOR PRECAST FOUNDATION UNITS - THE COVER OF CONCRETE OVER THE BOTTOM REINFORCEMENT SHALL BE 3 INCHES MINIMUM. THE COVER OF CONCRETE FOR ALL OTHER REINFORCEMENT SHALL BE 2 INCHES MINIMUM. THE CLEAR DISTANCE FROM THE END OF EACH PRECAST ELEMENT TO THE END OF REINFORCING STEEL SHALL NOT BE LESS THAN 2 INCHES NOR MORE THAN 3 INCHES. REINFORCEMENT SHALL BE ASSEMBLED UTILIZING A SINGLE LAYER OF WELDED WIRE FABRIC OR A SINGLE LAYER OF DEFORMED BILLET STEEL BARS. WELDED WIRE FABRIC SHALL MEET THE TRANSVERSE AND LONGITUDINAL WIRES MEETING THE SPACING REQUIREMENTS OF 4.3, BELOW, AND SHALL CONTAIN SUFFICIENT LONGITUDINAL WIRES EXTENDING THROUGH THE ELEMENT TO MAINTAIN THE SHAPE AND POSITION OF THE REINFORCEMENT. LONGITUDINAL REINFORCEMENT MAY BE WELDED WIRE FABRIC OR DEFORMED BILLET STEEL BARS AND SHALL MEET THE SPACING REQUIREMENTS OF 4.3, BELOW.
- 4.3. LAPS, WELDS, SPACING AND SPACING FOR PRECAST BRIDGE UNITS - TENSION SPLICES IN THE CIRCUMFERENTIAL REINFORCEMENT SHALL BE MADE BY LAPPING. LAPS MAY BE TACK WELDED TOGETHER FOR ASSEMBLY PURPOSES. FOR SMOOTH WELDED WIRE FABRIC, THE OVERLAP SHALL MEET THE REQUIREMENTS OF AASHTO 5.11.2.5.2 AND 5.11.2.5.4 AND 5.11.6.1. FOR DEFORMED BILLET STEEL BARS, THE OVERLAP SHALL MEET THE REQUIREMENTS OF AASHTO 5.11.2.1 FOR SPLICES OTHER THAN LAP SPLICES. THE OVERLAP SHALL BE A MINIMUM OF 1'0" FOR WELDED WIRE FABRIC OR DEFORMED BILLET STEEL BARS. THE SPACING CENTER TO CENTER OF THE CIRCUMFERENTIAL WIRES IN A WIRE FABRIC SHEET SHALL BE NOT LESS THAN 2" NOR MORE THAN 4". THE SPACING CENTER TO CENTER OF THE LONGITUDINAL WIRES SHALL NOT BE MORE THAN 6". THE SPACING CENTER TO CENTER OF THE LONGITUDINAL DISTRIBUTION STEEL FOR EITHER LINE OF REINFORCING IN THE TOP SLAB SHALL BE NOT MORE THAN 1'0".
- 4.3.2. LAPS, WELDS, AND SPACING FOR PRECAST WINGWALLS, HEADWALLS AND FOUNDATIONS - SPLICES IN THE REINFORCEMENT SHALL BE MADE BY LAPPING. LAPS MAY BE TACK WELDED TOGETHER FOR ASSEMBLY PURPOSES. FOR SMOOTH WELDED WIRE FABRIC, THE OVERLAP SHALL MEET THE REQUIREMENTS OF AASHTO 5.11.2.5.2 AND 5.11.6.2. FOR DEFORMED WELDED WIRE FABRIC, THE OVERLAP SHALL MEET THE REQUIREMENTS OF AASHTO 5.11.2.1 AND 5.11.6.1. FOR DEFORMED BILLET STEEL BARS, THE OVERLAP SHALL MEET THE REQUIREMENTS OF AASHTO 5.11.2.1. THE SPACING CENTER-TO-CENTER OF THE WIRES OF WIRE FABRIC SHALL BE NOT LESS THAN 2" NOR MORE THAN 6".
- 4.4. CURING - THE PRECAST CONCRETE ELEMENTS SHALL BE CURED FOR A SUFFICIENT LENGTH OF TIME TO MAINTAIN AND DEVELOPE THE SPECIFIED COMpressive STRENGTH IN 28 DAYS OR LESS, ANY ONE OF THE FOLLOWING METHODS OF CURING OR COMBINATIONS THEREOF SHALL BE USED.
- 4.4.1. STEAM CURING - THE PRECAST ELEMENTS MAY BE LOW-PRESSURE STEAM CURED BY A SYSTEM THAT WILL MAINTAIN A MOIST ATMOSPHERE.
- 4.4.2. WATER CURING - THE PRECAST ELEMENTS MAY BE WATER CURED BY ANY METHOD THAT WILL KEEP THE SECTIONS MOIST.
- 4.4.3. MEMBRANE CURING - A SEALING MEMBRANE CONFORMING TO THE REQUIREMENTS OF ASTM SPECIFICATION C309 MAY BE APPLIED AND SHALL BE LEFT INTACT UNTIL THE REQUIRED CONCRETE COMPRESSIVE STRENGTH IS ATTAINED. THE CONCRETE TEMPERATURE AT THE TIME OF APPLICATION SHALL BE WITHIN 4°-10 DEGREES F OF THE ATMOSPHERIC TEMPERATURE. ALL SURFACES SHALL BE KEPT MOIST PRIOR TO THE APPLICATION OF THE COMPOUNDS AND SHALL BE DAMP WHEN THE COMPOUND IS APPLIED.
- 4.5. STORAGE, HANDLING & DELIVERY**
- 4.5.1. STORAGE - PRECAST CONCRETE BRIDGE ELEMENTS SHALL BE LIFTED AND STORED IN THE CAST POSITION. PRECAST CONCRETE HEADWALL AND WINGWALL UNITS ARE CAST, STORED AND SHIPPED IN A FLAT POSITION. THE PRECAST ELEMENTS SHALL BE STORED IN SUCH A MANNER TO PREVENT CRACKING OR DAMAGE. STORE ELEMENTS USING TIMBER SUPPORTS AS APPROPRIATE. THE UNITS SHALL NOT BE MOVED UNTIL THE CONCRETE COMPRESSIVE STRENGTH HAS REACHED A MINIMUM OF 2600 PSI, AND THEY SHALL NOT BE STORED IN AN UPRIGHT POSITION.
- 4.5.2. HANDLING - HANDLING DEVICES SHALL BE PERMITTED IN EACH PRECAST ELEMENT FOR THE PURPOSE OF HANDLING AND SETTING. SPREADER BEAMS MAY BE REQUIRED FOR THE LIFTING OF PRECAST CONCRETE BRIDGE ELEMENTS TO PRECLUDE DAMAGE FROM BENDING OR TORSION FORCES.
- 4.5.3. DELIVERY - PRECAST CONCRETE ELEMENTS MUST NOT BE SHIPPED UNTIL THE CONCRETE HAS ATTAINED THE SPECIFIED DESIGN COMPRESSIVE STRENGTH, OR AS DIRECTED BY THE DESIGN ENGINEER. PRECAST CONCRETE ELEMENTS MAY BE UNLOADED AND PLACED ON THE GROUND AT THE SITE UNTIL INSTALLED. STORE ELEMENTS USING TIMBER SUPPORTS AS APPROPRIATE.
- 4.6. QUALITY ASSURANCE - THE PRECASTER SHALL DEMONSTRATE ADHERENCE TO THE STANDARDS SET FORTH IN THE NPCA QUALITY CONTROL MANUAL. THE PRECASTER SHALL MEET EITHER SECTION 4.6.1 OR 4.6.2.
- 4.6.1. CERTIFICATION - THE PRECASTER SHALL BE CERTIFIED BY THE PRECAST/RESTRESSED CONCRETE INSTITUTE PLANT CERTIFICATION PROGRAM OR THE NATIONAL PRECAST CONCRETE ASSOCIATIONS PLANT CERTIFICATION PROGRAM PRIOR TO AND DURING PRODUCTION OF THE PRODUCTS COVERED BY THIS SPECIFICATION.
- 4.6.2. THE PRECASTER SHALL HAVE BEEN IN THE BUSINESS OF PRODUCING PRECAST CONCRETE PRODUCTS SIMILAR TO THOSE SPECIFIED FOR A MINIMUM OF THREE YEARS. HE SHALL MAINTAIN A PERMANENT QUALITY CONTROL DEPARTMENT OR RETAIN AN INDEPENDENT TESTING AGENCY ON A CONTINUING BASIS. THE AGENCY SHALL ISSUE A REPORT, CERTIFIED BY A LICENSED ENGINEER, DETAILING THE TESTS PERFORMED BY THE PRECASTER TO PRODUCE QUALITY PRODUCTS CONSISTENT WITH INDUSTRY STANDARDS.
- 4.6.2.2. THE PRECASTER SHALL SHOW THAT THE FOLLOWING TESTS ARE PERFORMED IN ACCORDANCE WITH THE ASTM STANDARDS INDICATED. TESTS SHALL BE PERFORMED AS

- INDICATED IN SECTION 6 OF THESE SPECIFICATIONS.
- 4.6.2.2.1. AIR CONTENT - C231 OR C131
- 4.6.2.2.2. COMPRESSIVE STRENGTH - C139, C239, C497
- 4.6.2.3. THE PRECASTER SHALL PROVIDE DOCUMENTATION DEMONSTRATING COMPLIANCE WITH THIS SECTION TO CONTECH® ENGINEERED SOLUTIONS AT REGULAR INTERVALS OR UPON REQUEST.
- 4.6.2.4. THE OWNER MAY PLACE AN INSPECTOR IN THE PLANT WHEN THE PRODUCTS COVERED BY THIS SPECIFICATION ARE BEING MANUFACTURED.
- 4.6.3. DOCUMENTATION - THE PRECASTER SHALL SUBMIT PRECAST PRODUCTION REPORTS TO CONTECH® ENGINEERED SOLUTIONS AS REQUIRED.
- 5. PERMISSIBLE VARIATIONS**
- 5.1. BRIDGE UNITS
- 5.1.1. INTERNAL DIMENSIONS - THE INTERNAL DIMENSION SHALL VARY NOT MORE THAN 1% FROM THE DESIGN DIMENSIONS NOR MORE THAN 1/8" WHICHEVER IS LESS.
- 5.1.2. SLAB AND WALL THICKNESS - THE SLAB AND WALL THICKNESS SHALL NOT BE LESS THAN THAT SHOWN IN THE DESIGN BY MORE THAN X. A THICKNESS MORE THAN THAT REQUIRED IN THE DESIGN SHALL NOT BE CAUSE FOR REJECTION.
- 5.1.3. LENGTH OF OPPOSITE SURFACES - VARIATIONS IN LAYING LENGTHS OF TWO OPPOSITE SURFACES OF THE BRIDGE UNIT SHALL NOT BE MORE THAN IN ANY SECTION EXCEPT WHERE BEVELED ENDS FOR LAYING OF CURVES ARE SPECIFIED BY THE PURCHASER.
- 5.1.4. WIDTH OF SECTION - THE UNDERMIN LENGTH OF A SECTION SHALL NOT BE MORE THAN 2% IN ANY BRIDGE UNIT.
- 5.1.5. POSITION OF REINFORCEMENT - THE MAXIMUM VARIATION IN POSITION OF THE REINFORCEMENT SHALL BE 1/2" IN NO CASE SHALL THE COVER OVER THE EXTERNAL OR INTERNAL SURFACE OF THE BRIDGE. THESE TOLERANCES OR COVER REQUIREMENTS DO NOT APPLY TO MATING SURFACES OF THE BRIDGE UNITS.
- 5.1.6. AREA OF REINFORCEMENT - THE AREAS OF STEEL REINFORCEMENT SHALL BE THE DESIGN STEEL AREAS AS SHOWN IN THE MANUFACTURER'S SHOP DRAWINGS. STEEL AREAS GREATER THAN THOSE REQUIRED SHALL NOT BE CAUSE FOR REJECTION. THE PERMISSIBLE VARIATION IN DIAMETER OF ANY REINFORCING SHALL CONFORM TO THE TOLERANCES PRESCRIBED IN THE ASTM SPECIFICATION FOR THAT TYPE OF REINFORCEMENT.
- 5.2. WINGWALLS**
- 5.2.1. WALL THICKNESS - THE WALL THICKNESS SHALL NOT VARY FROM THAT SHOWN IN THE DESIGN BY MORE THAN 2%.
- 5.2.2. HEIGHT OF WALL SECTIONS - THE LENGTH AND HEIGHT OF THE WALL SHALL NOT VARY FROM THAT SHOWN IN THE DESIGN BY MORE THAN 2%.
- 5.2.3. POSITION OF REINFORCEMENT - THE MAXIMUM VARIATION IN THE POSITION OF THE REINFORCEMENT SHALL BE 1/2" IN NO CASE SHALL THE COVER OVER THE REINFORCEMENT BE LESS THAN 1/2".
- 5.2.4. DIAMETER OF ANY REINFORCING SHALL CONFORM TO THE TOLERANCES PRESCRIBED IN THE ASTM SPECIFICATION FOR THAT TYPE OF REINFORCING. STEEL AREA GREATER THAN THAT REQUIRED SHALL NOT BE CAUSE FOR REJECTION.
- 5.3. FOUNDATION UNITS**
- 5.3.1. WALL THICKNESS - THE WALL THICKNESS SHALL NOT VARY FROM THAT SHOWN IN THE DESIGN BY MORE THAN 2%.
- 5.3.2. LENGTH HEIGHT/WIDTH OF FOUNDATION SECTIONS - THE LENGTH, HEIGHT AND WIDTH OF THE FOUNDATION UNITS SHALL NOT VARY FROM THAT SHOWN IN THE DESIGN BY MORE THAN 2%.
- 5.3.3. POSITION OF REINFORCEMENT - THE MAXIMUM VARIATION IN THE POSITION OF THE REINFORCEMENT SHALL BE 1/2" IN NO CASE SHALL THE COVER OVER THE REINFORCEMENT BE LESS THAN 1/2".
- 5.3.4. SIZE OF REINFORCEMENT - THE PERMISSIBLE VARIATION IN DIAMETER OF ANY REINFORCING SHALL CONFORM TO THE TOLERANCES PRESCRIBED IN THE ASTM SPECIFICATION FOR THAT TYPE OF REINFORCING. STEEL AREA GREATER THAN THAT REQUIRED SHALL NOT BE CAUSE FOR REJECTION.
- 6. TESTING/INSPECTION**
- 6.1. TESTING
- 6.1.1. TYPE OF TEST SPECIMEN - CONCRETE COMPRESSIVE STRENGTH SHALL BE DETERMINED FROM COMPRESSION TESTS MADE ON CYLINDERS OR CORES. FOR EACH BRIDGE UNIT, A MINIMUM OF FOUR CYLINDERS SHALL BE TAKEN FOR EACH BRIDGE ELEMENT. EACH ELEMENT SHALL BE CONSIDERED SEPARATELY FOR THE PURPOSE OF TESTING AND ACCEPTANCE.
- 6.1.2. COMPRESSION TESTING - CYLINDERS SHALL BE MADE AND TESTED AS PRESCRIBED BY THE ASTM C39 SPECIFICATION. CYLINDERS SHALL BE CURED IN THE SAME ENVIRONMENT AS THE BRIDGE ELEMENTS. CORES SHALL BE OBTAINED AND TESTED FOR COMPRESSIVE STRENGTH IN ACCORDANCE WITH THE PROVISIONS OF THE ASTM C42 SPECIFICATION.
- 6.1.3. ACCEPTABILITY OF CYLINDER TESTS - WHEN THE AVERAGE COMPRESSIVE STRENGTH OF ALL CYLINDERS OF THE BRIDGE IS EQUAL TO OR GREATER THAN THE DESIGN COMPRESSIVE

- STRENGTH, AND NOT MORE THAN 10% OF THE CYLINDERS TESTED HAVE A COMPRESSIVE STRENGTH LESS THAN THE DESIGN CONCRETE STRENGTH, AND NO CYLINDER TESTED HAS A COMPRESSIVE STRENGTH LESS THAN 80% OF THE DESIGN CONCRETE STRENGTH, THEN THE ELEMENT SHALL BE ACCEPTED. WHEN THE COMPRESSIVE STRENGTH OF THE CYLINDERS TESTED DOES NOT CONFORM TO THESE ACCEPTANCE CRITERIA, THE ACCEPTABILITY OF THE ELEMENT MAY BE DETERMINED AS DESCRIBED IN SECTION 6.1.4, BELOW.
- 6.1.4. ACCEPTABILITY OF CORE TESTS - THE COMPRESSIVE STRENGTH OF THE CONCRETE IN A BRIDGE ELEMENT IS ACCEPTABLE WHEN THE AVERAGE CORE TEST STRENGTH IS EQUAL TO OR GREATER THAN THE DESIGN CONCRETE STRENGTH, WHEN THE COMPRESSIVE STRENGTH OF A CORE TESTED IS LESS THAN THE DESIGN CONCRETE STRENGTH, THE PRECAST ELEMENT FROM WHICH THAT CORE WAS TAKEN MAY BE RE-CORED, WHEN THE COMPRESSIVE STRENGTH OF THE RE-CORE IS EQUAL TO OR GREATER THAN THE DESIGN CONCRETE STRENGTH, THE COMPRESSIVE STRENGTH OF THE CONCRETE IN THAT BRIDGE ELEMENT IS ACCEPTABLE.
- 6.1.4.1. WHEN THE COMPRESSIVE STRENGTH OF ANY CORE IS LESS THAN THE DESIGN CONCRETE STRENGTH, THE PRECAST ELEMENT FROM WHICH THAT CORE WAS TAKEN SHALL BE REJECTED.
- 6.1.4.2. PLUGGING CORE HOLES - THE CORE HOLES SHALL BE PLUGGED AND SEALED BY THE MANUFACTURER IN A MANNER SUCH THAT THE ELEMENTS WILL MEET ALL OF THE TEST REQUIREMENTS OF THIS SPECIFICATION. PRECAST ELEMENTS SO SEALED SHALL BE CONSIDERED SATISFACTORY FOR USE.
- 6.1.4.3. TEST EQUIPMENT - EVERY MANUFACTURER FURNISHING PRECAST ELEMENTS UNDER THIS SPECIFICATION SHALL FURNISH ALL FACILITIES AND PERSONNEL NECESSARY TO CARRY OUT THE TEST REQUIRED.
- 6.2. INSPECTION - THE QUALITY OF MATERIALS, THE PROCESS OF MANUFACTURE, AND THE FINISHED PRECAST ELEMENTS SHALL BE SUBJECT TO INSPECTION BY THE PURCHASER.
- 7. JOINTS**
- THE BRIDGE UNITS SHALL BE PRODUCED WITH FLAT BUTT ENDS. THE ENDS OF THE BRIDGE UNITS SHALL BE SUCH THAT WHEN THE SECTIONS ARE LAID TOGETHER THEY WILL MAKE A CONTINUOUS LINE WITH A SMOOTH INTERIOR FREE OF APPRECIABLE IRREGULARITIES, ALL COMPATIBLE WITH THE PERMISSIBLE ADJUSTMENTS IN SECTION 5, ABOVE. THE JOINT WIDTH BETWEEN ADJACENT PRECAST UNITS SHALL NOT EXCEED 1/2".
- 8. WORKMANSHIP FINISH**
- THE BRIDGE UNITS, WINGWALLS, HEADWALLS AND FOUNDATION UNITS SHALL BE SUBSTANTIALLY FREE OF FRACTURES. THE ENDS OF THE BRIDGE UNITS SHALL BE NORMAL TO THE WALLS AND CENTERLINE OF THE BRIDGE SECTION. WITHIN THE LIMITS OF THE VARIATIONS GIVEN IN SECTION 5, ABOVE, EXCEPT WHERE BEVELED ENDS ARE SPECIFIED, THE ENDS OF THE WINGWALLS AND HEADWALLS SHALL BE PARALLEL TO EACH OTHER, WITHIN THE LIMITS OF VARIATIONS GIVEN IN SECTION 5, ABOVE. THE SURFACE OF THE PRECAST ELEMENTS SHALL BE A SMOOTH STEEL FORM FINISHED TROVELED SURFACE. TRAPPED AIR POCKETS CAUSING SURFACE DEFECTS SHALL BE CONSIDERED AS PART OF A SMOOTH, STEEL FORM FINISH.
- 9. REPAIRS**
- PRECAST ELEMENTS MAY BE REPAIRED, IF NECESSARY, BECAUSE OF IMPERFECTIONS IN MANUFACTURE OR HANDLING DAMAGE AND WILL BE ACCEPTABLE IF, IN THE OPINION OF THE PURCHASER, THE REPAIRS ARE SOUND, PROPERLY FINISHED AND CURED, AND THE REPAIRED SECTION CONFORMS TO THE REQUIREMENTS OF THIS SPECIFICATION.
- 10. REJECTION**
- THE PRECAST ELEMENTS SHALL BE SUBJECT TO REJECTION ON ACCOUNT OF ANY OF THE SPECIFICATION REQUIREMENTS. INDIVIDUAL PRECAST ELEMENTS MAY BE REJECTED BECAUSE OF ANY OF THE FOLLOWING:
- 10.1. FRACTURES OR CRACKS PASSING THROUGH THE WALL, EXCEPT FOR A SINGLE END CRACK THAT DOES NOT EXCEED ONE HALF THE THICKNESS OF THE WALL.
- 10.2. DEFECTS THAT INDICATE PROPORTIONING, MIXING, AND MOLDING NOT IN COMPLIANCE WITH SECTION 4 OF THESE SPECIFICATIONS.
- 10.3. HONEYCOMBED OR OPEN TEXTURE.
- 10.4. DAMAGED ENDS, WHERE SUCH DAMAGE WOULD PREVENT MAKING A SATISFACTORY JOINT.

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CONSPAN
BRIDGE SYSTEMS

FABRICATION
DRAWING

NAPLES RESERVE CIRCLE BRIDGE
COLLIER COUNTY, FLORIDA

PROJECT NO.	SEQ. NO.	DATE
513323	010	9/19/2017
DESIGNED	MGC	DRAWN ALK
CHECKED	KMH	APPROVED MGC
SHEET NO.	S17 OF S18	

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SPECIFICATIONS FOR MANUFACTURE AND INSTALLATION OF CON/SPAN® BRIDGE SYSTEMS (CONT'D)

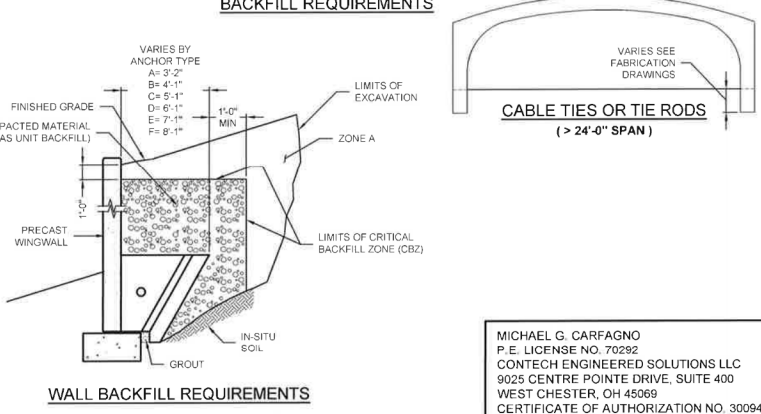
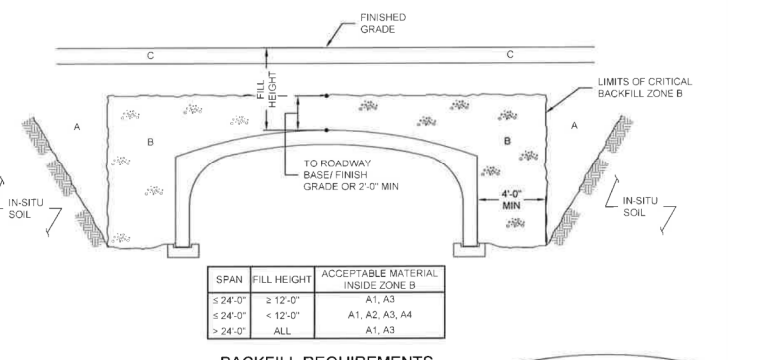
- 11. MARKING**
EACH BRIDGE UNIT SHALL BE CLEARLY MARKED BY WATERPROOF PAINT. THE FOLLOWING SHALL BE SHOWN ON THE INSIDE OF THE VERTICAL LEG OF THE BRIDGE SECTION:
BRIDGE SPAN x BRIDGE RISE
DATE OF MANUFACTURE
NAME OR TRADEMARK OF THE MANUFACTURER
- 12. INSTALLATION PREPARATION**
TO ENSURE CORRECT INSTALLATION OF THE PRECAST CONCRETE BRIDGE SYSTEM, CARE AND CAUTION MUST BE EXERCISED IN FORMING THE SUPPORT AREAS FOR BRIDGE UNITS, HEADWALL, AND WINGWALL ELEMENTS. EXERCISING SPECIAL CARE WILL FACILITATE THE RAPID INSTALLATION OF THE PRECAST COMPONENTS.
- 12.1. FOOTINGS**
DO NOT OVER EXCAVATE FOUNDATIONS UNLESS DIRECTED BY SITE SOIL ENGINEER TO REMOVE UNSUITABLE SOIL.
THE SITE SOILS ENGINEER SHALL CERTIFY THAT THE BEARING CAPACITY MEETS OR EXCEEDS THE FOOTING DESIGN REQUIREMENTS, PRIOR TO THE CONTRACTOR POURING OF THE FOOTINGS.
THE BRIDGE UNITS AND WINGWALLS SHALL BE INSTALLED ON EITHER PRECAST OR CAST-IN-PLACE CONCRETE FOOTINGS. THE SIZE AND ELEVATION OF THE FOOTINGS SHALL BE AS DESIGNED BY THE ENGINEER. A KEYWAY SHALL BE FORMED IN THE TOP SURFACE OF THE BRIDGE FOOTING AS SPECIFIED ON THE PLANS. NO KEYWAY IS REQUIRED IN THE WINGWALL FOOTINGS, UNLESS OTHERWISE SPECIFIED ON THE PLANS.
THE FOOTINGS SHALL BE GIVEN A SMOOTH FLOAT FINISH AND SHALL REACH A COMPRESSIVE STRENGTH OF 2,000 PSI BEFORE PLACEMENT OF THE BRIDGE AND WINGWALL ELEMENTS. BACKFILLING SHALL NOT BEGUN UNTIL THE FOOTING HAS REACHED THE FULL DESIGN COMPRESSIVE STRENGTH.
THE FOOTING SURFACE SHALL BE CONSTRUCTED IN ACCORDANCE WITH GRADES SHOWN ON THE PLANS, WHEN TESTED WITH A 10'-0" STRAIGHT EDGE, THE SURFACE SHALL NOT VARY MORE THAN 1/4" IN 10'-0".
IF A PRECAST CONCRETE FOOTING IS USED, THE CONTRACTOR SHALL PREPARE A 4" THICK BASE LAYER OF COMPACTED GRANULAR MATERIAL THE FULL WIDTH OF THE FOOTING PRIOR TO PLACING THE PRECAST FOOTING.
THE FOUNDATIONS FOR PRECAST CONCRETE BRIDGE ELEMENTS AND WINGWALLS MUST BE CONNECTED BY REINFORCEMENT TO FORM ONE MONOLITHIC BODY. EXPANSION JOINTS SHALL NOT BE USED.
THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONSTRUCTION OF THE FOUNDATIONS PER THE PLANS AND SPECIFICATIONS.
- 13. INSTALLATION**
- 13.1. GENERAL** - THE INSTALLATION OF THE PRECAST CONCRETE ELEMENTS SHALL BE PLANNED IN THE PUBLICATION CONSPAN BRIDGE SYSTEMS INSTALLATION HANDBOOK.
- 13.1.1. LIFTING** - IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT A GRADE OF THE CORRECT LIFTING CAPACITY IS AVAILABLE TO HANDLE THE PRECAST CONCRETE UNITS. THIS CAN BE ACCOMPLISHED BY USING THE WEIGHTS GIVEN FOR THE PRECAST CONCRETE COMPONENTS AND BY DETERMINING THE LIFTING REACH FOR EACH CRANE UNIT. SITE CONDITIONS MUST BE CHECKED WELL IN ADVANCE OF SHIPPING TO ENSURE PROPER CRANE LOCATION AND TO AVOID ANY LIFTING RESTRICTIONS. THE LIFT ANCHORS OR HOLES PROVIDED IN EACH UNIT ARE THE ONLY MEANS TO BE USED TO LIFT THE ELEMENTS. THE PRECAST CONCRETE ELEMENTS MUST NOT BE SUPPORTED OR RAISED BY OTHER MEANS THAN THOSE GIVEN IN THE MANUALS AND DRAWINGS WITHOUT WRITTEN APPROVAL FROM CONTECH® ENGINEERED SOLUTIONS.
- 13.1.2. CONSTRUCTION EQUIPMENT WEIGHT RESTRICTIONS** - IN NO CASE SHALL EQUIPMENT OPERATING IN EXCESS OF THE DESIGN LOAD (HS-20) BE PERMITTED OVER THE BRIDGE UNITS UNLESS APPROVED BY CONTECH® ENGINEERED SOLUTIONS.
- 13.1.2.1. IN THE IMMEDIATE AREA OF THE BRIDGE UNITS**, THE FOLLOWING RESTRICTIONS FOR THE USE OF HEAVY CONSTRUCTION MACHINERY DURING BACKFILLING OPERATIONS APPLY.
• NO CONSTRUCTION EQUIPMENT SHALL CROSS THE BARE PRECAST CONCRETE BRIDGE UNIT.
• AFTER THE COMPACTED FILL LEVEL HAS REACHED A MINIMUM OF 4" OVER THE CROWN OF THE BRIDGE, CONSTRUCTION EQUIPMENT WITH A WEIGHT OF LESS THAN 10 TONS MAY CROSS THE BRIDGE.
• AFTER THE COMPACTED FILL LEVEL HAS REACHED A MINIMUM OF 1'-0" OVER THE CROWN OF THE BRIDGE, CONSTRUCTION EQUIPMENT WITH A WEIGHT OF LESS THAN 30 TONS MAY CROSS THE BRIDGE.
• AFTER THE COMPACTED FILL LEVEL HAS REACHED THE DESIGN COVER, OR 2'-0" MINIMUM, OVER THE CROWN OF THE PRECAST CONCRETE BRIDGE, CONSTRUCTION EQUIPMENT WITHIN THE DESIGN LOAD LIMITS FOR THE ROAD MAY CROSS THE PRECAST CONCRETE BRIDGE.
- 13.2. LEVELING PADS/SHIMS** - THE BRIDGE UNITS AND WINGWALLS SHALL BE SET ON HARDWOOD SHIMS CONFORMING TO ASTM D1037 OR PLASTIC SHIMS (DAYTON SUPERIOR #80, P-61 OR EQUIVALENT) EQUAL MEASURING 5" x 5" MINIMUM, UNLESS SHOWN OTHERWISE ON THE PLANS. A MINIMUM GAP OF 1/2" SHALL BE PROVIDED BETWEEN THE FOOTING AND THE BOTTOM OF THE BRIDGES

- VERTICAL LEGS OR THE BOTTOM OF THE WINGWALL, ALSO, A SURVEY OF 1/2" x 1/2" AND 1/2" THICK HARDWOOD OR PLASTIC SHIMS FOR VARIOUS SPACING PURPOSES SHALL BE ON SITE.
- 13.3. PLACEMENT OF BRIDGE UNITS** - THE BRIDGE UNITS SHALL BE PLACED AS SHOWN ON THE ENGINEER'S PLAN DRAWINGS. SPECIAL CARE SHALL BE TAKEN IN SETTING THE ELEMENTS TO THE TRUE LINE AND GRADE. THE JOINT WIDTH BETWEEN ADJACENT PRECAST UNITS SHALL NOT EXCEED 1/2".
- 13.4. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE STRUCTURE SPAN DURING ALL PHASES OF INSTALLATION.** DUE TO THE ARCH SHAPE, BRIDGE ELEMENTS WILL TEND TO SPREAD UNDER SELF-WEIGHT. IT IS IMPERATIVE THAT ANY LATERAL SPREADING OF THE BRIDGE ELEMENTS BE AVOIDED DURING AND AFTER THEIR PLACEMENT. GENERALLY, HORIZONTAL CABLE TIES OR TIE RODS ARE SHIPPED IN THE LARGER BRIDGE ELEMENTS TO ASSIST IN PREVENTING THIS SPREADING. CABLE TIE/TIE RODS SHALL NOT BE REMOVED UNTIL BRIDGE UNITS ARE GROUTED AND GROUT HAS CURED. IT IS RECOMMENDED THAT TEMPORARY HARDWOOD BLOCKS BE USED IN CONJUNCTION WITH THE CABLE TIE/TIE RODS TO MAINTAIN SPAN. IF, HOWEVER, DUE TO SITE RESTRICTIONS, THESE CABLE TIE/TIE RODS MUST BE REMOVED PRIOR TO PLACEMENT OF THE BRIDGE ELEMENTS, THE CONTRACTOR MUST NOTIFY CONTECH (MANUFACTURER) AND REQUEST A SUGGESTED INSTALLATION PROCEDURE.
IN ADDITION, IF THE CABLE TIE/TIE RODS MUST BE REMOVED PRIOR TO SETTING ARCH UNITS, THE FOLLOWING QUALITY CONTROL PROCEDURE MUST BE FOLLOWED:
1) FIND "MEASURED SPAN" UPON RECEIVING DELIVERY TO SITE. PRIOR TO LIFTING FROM TRUCK AND REMOVING CABLE TIE/TIE RODS, "MEASURED SPAN" SHALL BE THE AVERAGE OF (3) SPAN MEASUREMENTS ALONG THE FULL LENGTH OF THE ARCH UNIT.
2) AFTER SETTING OF BRIDGE UNIT ON THE FOUNDATION, VERIFY THE SPAN. THIS "INSTALLED SPAN MEASUREMENT" SHALL NOT EXCEED THE MAXIMUM OF:
A) THE NOMINAL SPAN "N" OR
B) THE "MEASURED SPAN".
IF THE "INSTALLED SPAN MEASUREMENT" EXCEEDS THIS AMOUNT, THE ARCH UNIT SHALL BE LIFTED AND RE-SET UNTIL THE "INSTALLED SPAN MEASUREMENT" MEETS THE LIMITS.
- 13.5. PLACEMENT OF WINGWALLS, HEADWALLS AND FOUNDATION UNITS** THE WINGWALLS, HEADWALLS AND FOUNDATIONS SHALL BE PLACED AS SHOWN ON THE PLAN DRAWINGS. SPECIAL CARE SHALL BE TAKEN IN SETTING THE ELEMENTS TO THE TRUE LINE AND GRADE.
- 13.6. WATERPROOFING/Joint PROTECTION AND SUBSURFACE DRAINAGE**
- 13.6.1. EXTERNAL PROTECTION OF JOINTS** - THE JOINT MADE BY TWO ADJOINING BRIDGE UNITS SHALL BE COVERED WITH A 1/2" x 1/2" REFORMED BITUMINOUS JOINT SEALANT AND A MINIMUM OF A 9" WIDE JOINT WRAP. THE SURFACE SHALL BE FREE OF DIRT BEFORE APPLYING THE JOINT MATERIAL. A PRIMER COMPATIBLE WITH THE JOINT WRAP TO BE USED SHALL BE APPLIED FOR A MINIMUM WIDTH OF 9" ON EACH SIDE OF THE JOINT. THE EXTERNAL WRAP SHALL BE CS212 BY CONCRETE SEALANTS INC. EZ-WRAP RUBBER BY PRESS SEAL, GASKET CORPORATION, SEAL WRAP BY MAR MAC MANUFACTURING CO. INC. OR APPROVED EQUAL. THE JOINT SHALL BE COVERED CONTIGUOUSLY FROM THE BOTTOM OF ONE BRIDGE SECTION LEG, ACROSS THE TOP OF THE BRIDGE AND TO THE OPPOSITE BRIDGE SECTION LEG. ANY LAPS THAT RESULT IN THE "JOINT WRAP" SHALL BE A MINIMUM OF 6" LONG WITH THE OVERLAP RUNNING DOWN.
- 13.6.2. IN ADDITION TO THE JOINTS BETWEEN BRIDGE UNITS, THE JOINT BETWEEN THE END BRIDGE UNIT AND THE HEADWALL SHALL ALSO BE SEALED AS DESCRIBED ABOVE. IF PRECAST WINGWALLS ARE USED, THE JOINT BETWEEN THE END BRIDGE UNIT AND THE WINGWALL SHALL BE SEALED WITH A 2'-0" STRIP OF FILTER FABRIC. ALSO, IF LIFT HOLES ARE FORMED IN THE BRIDGE UNITS, THEY SHALL BE PRIMED AND COVERED WITH A 9" x 9" SQUARE OF JOINT WRAP.**
- 13.6.3. DURING THE BACKFILLING OPERATION, CARE SHALL BE TAKEN TO KEEP THE JOINT WRAP IN ITS PROPER LOCATION OVER THE JOINT.**
- 13.6.4. SUBSOIL DRAINAGE SHALL BE AS DIRECTED BY THE ENGINEER.**
- 13.7. GROUTING**
- 13.7.1. GROUTING SHALL NOT BE PERFORMED WHEN TEMPERATURES ARE EXPECTED TO GO BELOW 35° FOR A PERIOD OF 72 HOURS** FROM THE BRIDGE FOUNDATION KEYWAY WITH CEMENT GROUT (PORTLAND CEMENT AND WATER OR CEMENT MORTAR COMPOSED OF PORTLAND CEMENT, SAND AND WATER) WITH A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 3000 PSI. WATER CEMENT GROUT MUST ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 1500 PSI BEFORE TIES MAY BE REMOVED.
- 13.7.2. ALL GROUT SHALL HAVE A MAXIMUM AGGREGATE SIZE OF 1/4".**
- 13.7.3. LIFTING AND ERECTION ANCHOR RECESSES SHALL BE FILLED WITH GROUT.**
- 13.7.4. AFTER GROUT HAS REACHED ITS DESIGN STRENGTH THE TEMPORARY HARDWOOD WEDGES SHALL BE REMOVED AND THEIR HOLES FILLED WITH GROUT.**
- 13.8. BACKFILLING**
- 13.8.1. DO NOT PERFORM BACKFILLING DURING WET OR FREEZING WEATHER.**

- 13.8.2. NO BACKFILL SHALL BE PLACED AGAINST ANY STRUCTURAL ELEMENTS UNTIL THEY HAVE BEEN APPROVED BY THE ENGINEER.**
- 13.8.3. BACKFILL SHALL BE CONSIDERED AS ALL REPLACED EXCAVATION AND NEW EMBANKMENT ADJACENT TO THE PRECAST CONCRETE ELEMENTS.** THE PROJECT CONSTRUCTION AND MATERIAL SPECIFICATIONS, WHICH INCLUDE THE SPECIFICATIONS FOR EXCAVATION FOR STRUCTURES AND ROADWAY EXCAVATION AND EMBANKMENT CONSTRUCTION, SHALL APPLY EXCEPT AS MODIFIED IN THIS SECTION.
- 13.8.4. BACKFILL ZONES**
• IN-SITU SOIL
• ZONE A: CONSTRUCTED EMBANKMENT OR OVERFILL.
• ZONE B: FILL THAT IS DIRECTLY ASSOCIATED WITH PRECAST CONCRETE BRIDGE INSTALLATION.
• ZONE C: ROAD STRUCTURE.
- 13.8.5. REQUIRED BACKFILL PROPERTIES**
- 13.8.5.1. IN-SITU SOIL** - NATURAL GROUND IS TO BE SUFFICIENTLY STABLE TO ALLOW EFFECTIVE SUPPORT TO THE PRECAST CONCRETE BRIDGE UNITS. AS A GUIDE, THE EXISTING NATURAL GROUND SHOULD BE OF SIMILAR QUALITY AND DENSITY TO ZONE B MATERIAL. FOR MINIMUM LATERAL DIMENSION OF ONE BRIDGE SPAN OUTSIDE OF THE BRIDGE FOOTING.
- 13.8.5.2. ZONE A** - ZONE A REQUIRES FILL MATERIAL WITH SPECIFICATIONS AND COMPACTING PROCEDURES EQUAL TO THAT FOR NORMAL ROAD EMBANKMENTS.
- 13.8.5.3. ZONE B** - GENERALLY, SOILS SHALL BE REASONABLY FREE OF ORGANIC MATTER, AND, NEAR CONCRETE SURFACES, FREE OF STONES LARGER THAN 3" IN DIAMETER. SEE CHARTS FOR DETAILED DESCRIPTIONS OF ACCEPTABLE SOILS.
- 13.8.5.4. ZONE C** - ZONE C IS THE ROAD SECTION OF GRAVEL, ASPHALT OR CONCRETE BUILT IN COMPLIANCE WITH LOCAL ENGINEERING PRACTICES.
- 13.8.5.5. GEOTECHNICAL ENGINEER SHALL REVIEW GRADATIONS OF ALL INTERFACING MATERIALS AND, IF NECESSARY, RECOMMEND GEOTEXTILE FILTER FABRIC (PROVIDED BY CONTRACTOR).**
- 13.8.6. PLACING AND COMPACTING BACKFILL**
DUMPING FOR BACKFILLING IS NOT ALLOWED ANY NEARER THAN 3'-0" FROM THE BRIDGE LEG.
THE FILL MUST BE PLACED AND COMPACTED IN LAYERS NOT EXCEEDING 8" THE MAXIMUM DIFFERENCE IN THE ELEVATION LEVELS OF THE FILL ON OPPOSITE SIDES OF THE BRIDGE MUST NOT EXCEED 2'-0".
THE FILL BEHIND WINGWALLS MUST BE PLACED AT THE SAME TIME AS THAT OF THE BRIDGE FILL. IT MUST BE PLACED IN PROGRESSIVELY PLACED HORIZONTAL LAYERS NOT EXCEEDING 8" PER LAYER.
THE BACKFILL OF ZONE B SHALL BE COMPACTED TO A MINIMUM DENSITY OF 95% OF THE STANDARD PROCTOR, AS REQUIRED BY AASHTO T-99.
SOIL WITHIN 1'-0" OF CONCRETE SURFACES SHALL BE HAND COMPACTED. ELSEWHERE, USE OF ROLLERS IS ACCEPTABLE. IF VIBRATING ROLLER COMPACTORS ARE USED, THEY SHALL NOT BE STARTED OR STOPPED WITHIN ZONE B AND THE VIBRATION FREQUENCY SHALL BE AT LEAST 30 REVOLUTIONS PER SECOND.
THE BACKFILL MATERIAL AND COMPACTING BEHIND WINGWALLS SHALL SATISFY THE CRITERIA FOR THE BRIDGE BACKFILL, ZONE B.
BACKFILL AGAINST A WATERPROOFED SURFACE SHALL BE PLACED CAREFULLY TO AVOID DAMAGE TO THE WATERPROOFING MATERIAL.
- 13.8.7. BRIDGE UNITS**
FOR FILL HEIGHTS OVER 12 FEET (AS MEASURED FROM TOP CROWN OF BRIDGE TO FINISHED GRADE), NO BACKFILLING MAY BEGIN UNTIL A BACKFILL COMPACTION TESTING PLAN HAS BEEN COORDINATED WITH AND APPROVED BY CONTECH® ENGINEERED SOLUTIONS.
- 13.8.8. WINGWALLS**
BACKFILL IN FRONT OF WINGWALLS SHALL BE CARRIED TO GROUND LINES SHOWN IN THE PLANS.
- 13.8.9. MONITORING**
THE CONTRACTOR SHALL CHECK SETTLEMENTS AND HORIZONTAL DISPLACEMENT OF FOUNDATION TO ENSURE THAT THEY ARE WITHIN THE ALLOWABLE LIMIT PROVIDED BY THE ENGINEER. THESE MEASUREMENTS SHOULD GIVE AN INDICATION OF THE SETTLEMENTS AND DEFORMATIONS ALONG THE LENGTH OF THE FOUNDATIONS.
THE FIRST MEASUREMENT SHOULD TAKE PLACE AFTER THE ERECTION OF ALL PRECAST BRIDGE SYSTEM ELEMENTS, A SECOND AFTER COMPLETION OF BACKFILLING, AND A THIRD BEFORE OPENING OF THE BRIDGE TO TRAFFIC. FURTHER MEASUREMENTS MAY BE MADE ACCORDING TO LOCAL CONDITIONS.

ACCEPTABLE SOILS FOR USE IN ZONE B BACKFILL

TYPICAL MATERIALS	AASHTO GROUP	AASHTO SUBGROUP	PI RECENT PASSING US SIEVE NO.		CHARACTER OF FRACTION PASSING NO. 40 SIEVE		SOIL DESCRIPTION	
			#10	#200	LIQUID LIMIT	PLASTICITY INDEX		
GW, GP, SP	A-1	A-1a	50 MAX	30 MAX	15 MAX	6 MAX	LARGELY GRAVEL BUT CAN INCLUDE SAND AND FINES	
GM, SW, SP, SM	A-1	A-1b	50 MAX	30 MAX	25 MAX	6 MAX	GRAVELLY SAND OR GRADED SAND, MAY INCLUDE FINES	
GM, SM, ML, SP, GP	A-2	A-2.4			35 MAX	40 MAX	10 MAX	SANDS, GRAVELS WITH LOW-PLASTICITY SILT FINES
SC, GC, GM	A-2	A-2.5			35 MAX	41 MIN	10 MAX	SANDS, GRAVELS WITH PLASTIC SILT FINES
SP, SM, SW	A-3			51 MIN	10 MAX			FINE SANDS
ML, SM, SC	A-4				36 MIN	40 MAX	10 MAX	LOW-COMPRESSIBILITY SILTS



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